



Program

Welcome by Pex Langenberg, Transport Counselor, Dutch Embassy

Remarks from Congressmen Earl Blumentauer, Tom Petry

Remarks from Gabe Klein, DDOT

Summary of the teams by

- Team Orange
- Team Blue
- General remarks

Q&A

Closing words by Pex Langenberg and Jim Sebastian, DDOT

10 Episodes on 2-Disc Set



From Executive Producers
Team Orange

L&M STREET

—The Complete Series—

Politics from the inside out.

ThinkBike



U.S. vs. Netherlands

American trip patterns are not dramatically longer than the Dutch

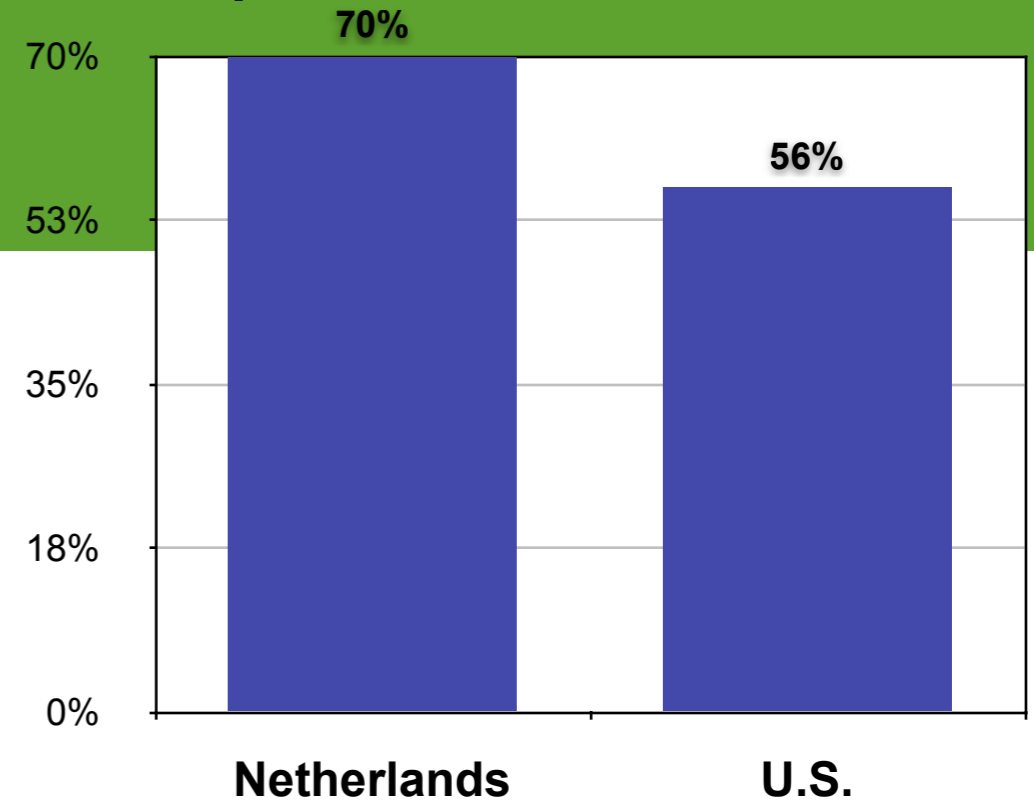
Most trips are less than 4 miles long

20 minutes by bike!

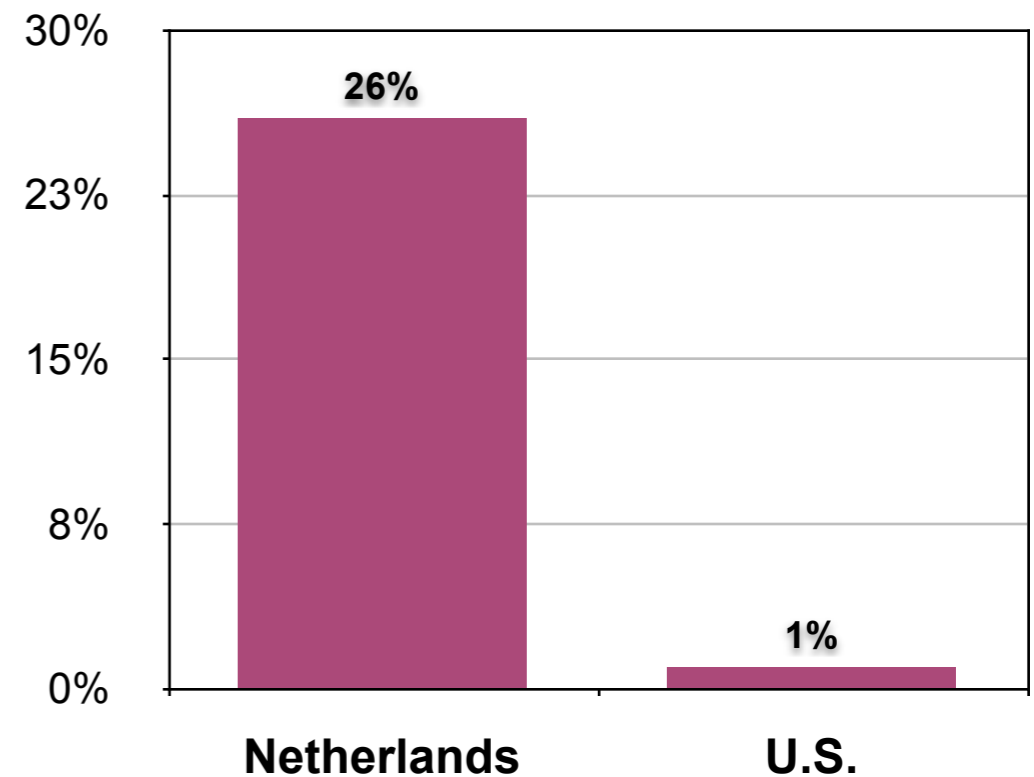
But bike use in the U.S. is *much* less

WHY?

Trips Less Than 4 miles/7.5 km



Bicycle Mode Share





Policy Differences between DC and Netherlands

Cost of getting a drivers license

Price of gasoline

Price/availability of parking

Excise taxes on automobile purchases

Lack of buses to school



Goals/recommendations for the workshop

- › Apply Dutch principles of bicycle facility design to Downtown Washington, DC
- › Design high quality bikeway for the downtown
 - Continuous
 - Signature “marquee” facility
 - Bi-directional
- › Bikeway as a transportation connection
- › Focus on bikeway design for L and M Streets NW, between Metropolitan Branch Trail and Georgetown
- › Extend livability principles beyond bikeways linking neighborhoods, retail, and economic development



Project: Creating the Crosstown Bikeway as a Regional Transportation Connection

- › L and M Streets
- › From Met Branch trail to Georgetown





Workshop process

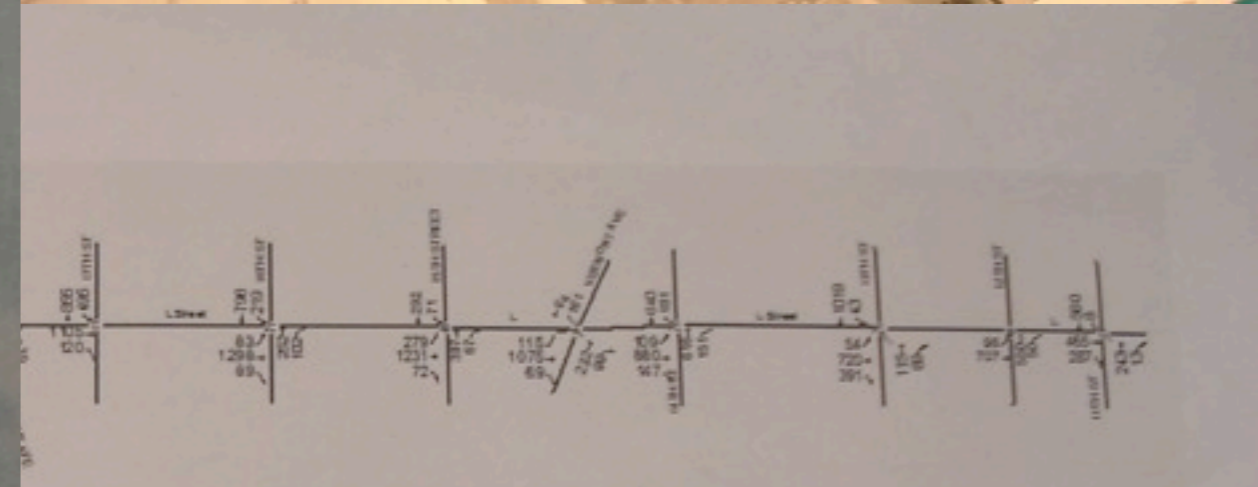
Presentations by Dutch experts

Surveyed L & M Streets

Reviewed maps

Brainstormed

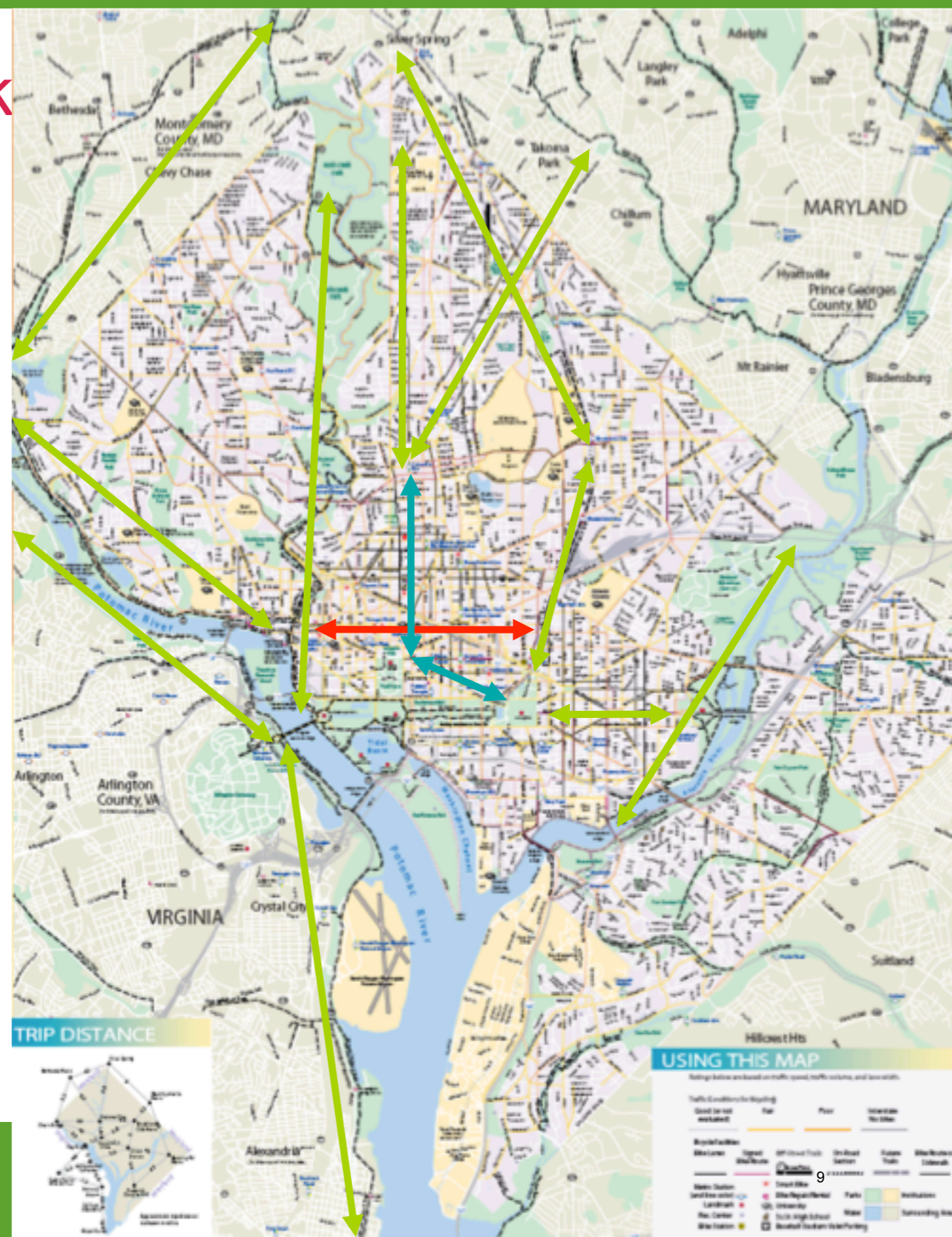
Created design treatments





Integrated Downtown Network

- › Design focuses on L & M Street
- › Key piece to create an integrated network:
 - North-Souths
 - Diagonals (e.g., Mass. Ave., Connecticut Ave.)
- › L & M should be part of an overall bikeway network throughout city
- › L&M can be pilot projects
 - Create a larger network,
 - Set robust design precedents





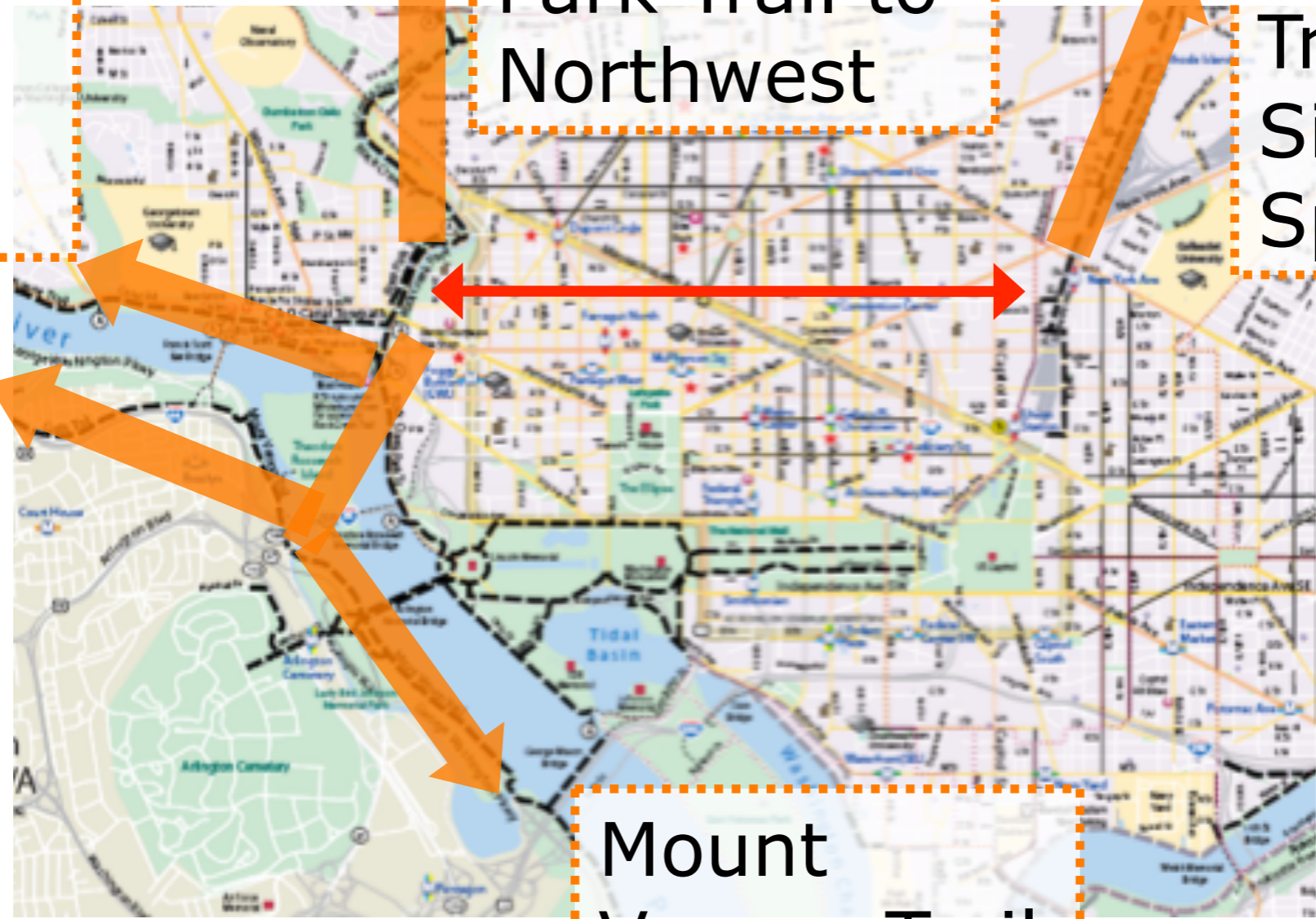
Key Connections to Regional Trail Network

Capital Crescent and C&O Trails

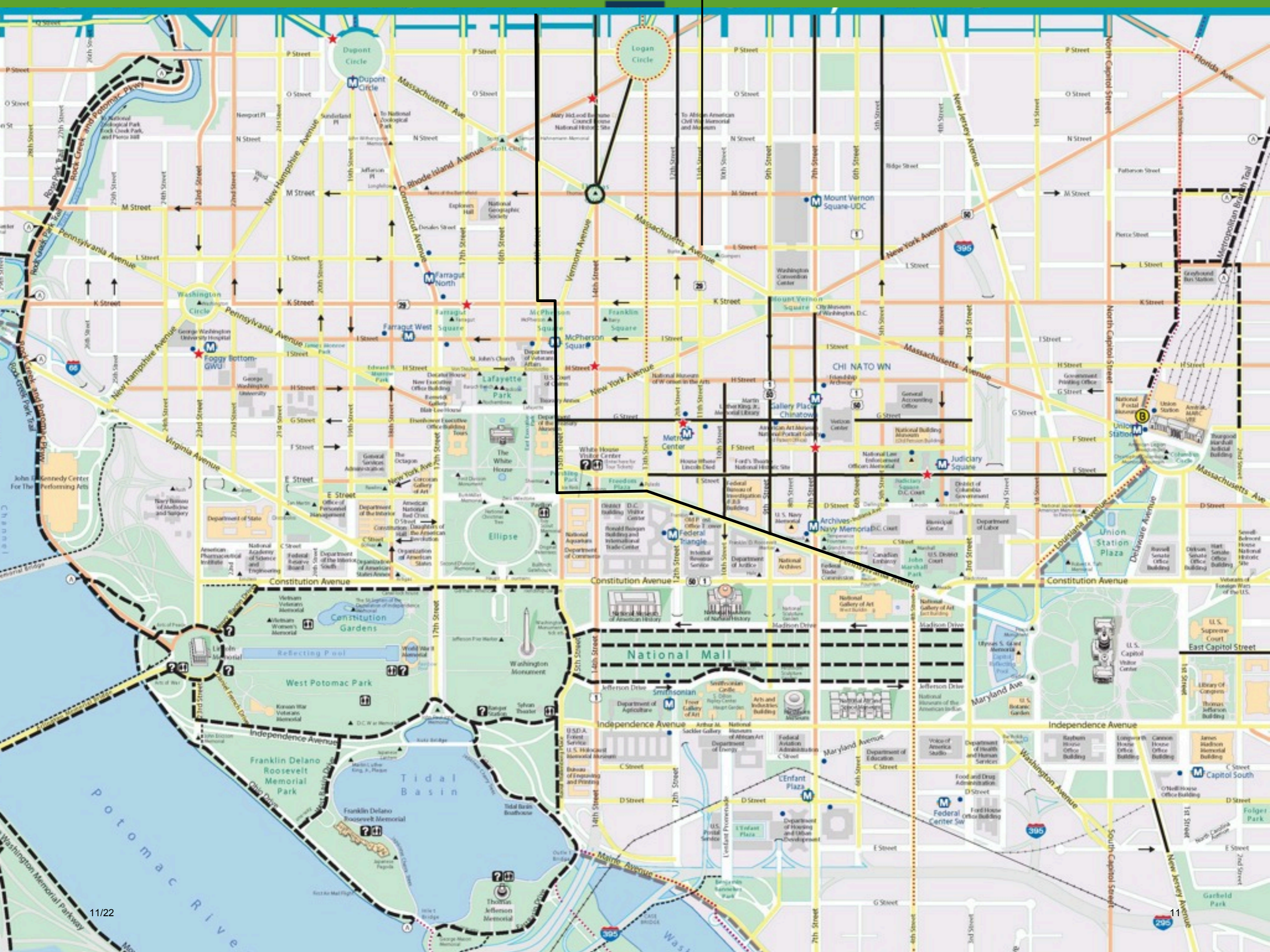
Rock Creek Park Trail to Northwest

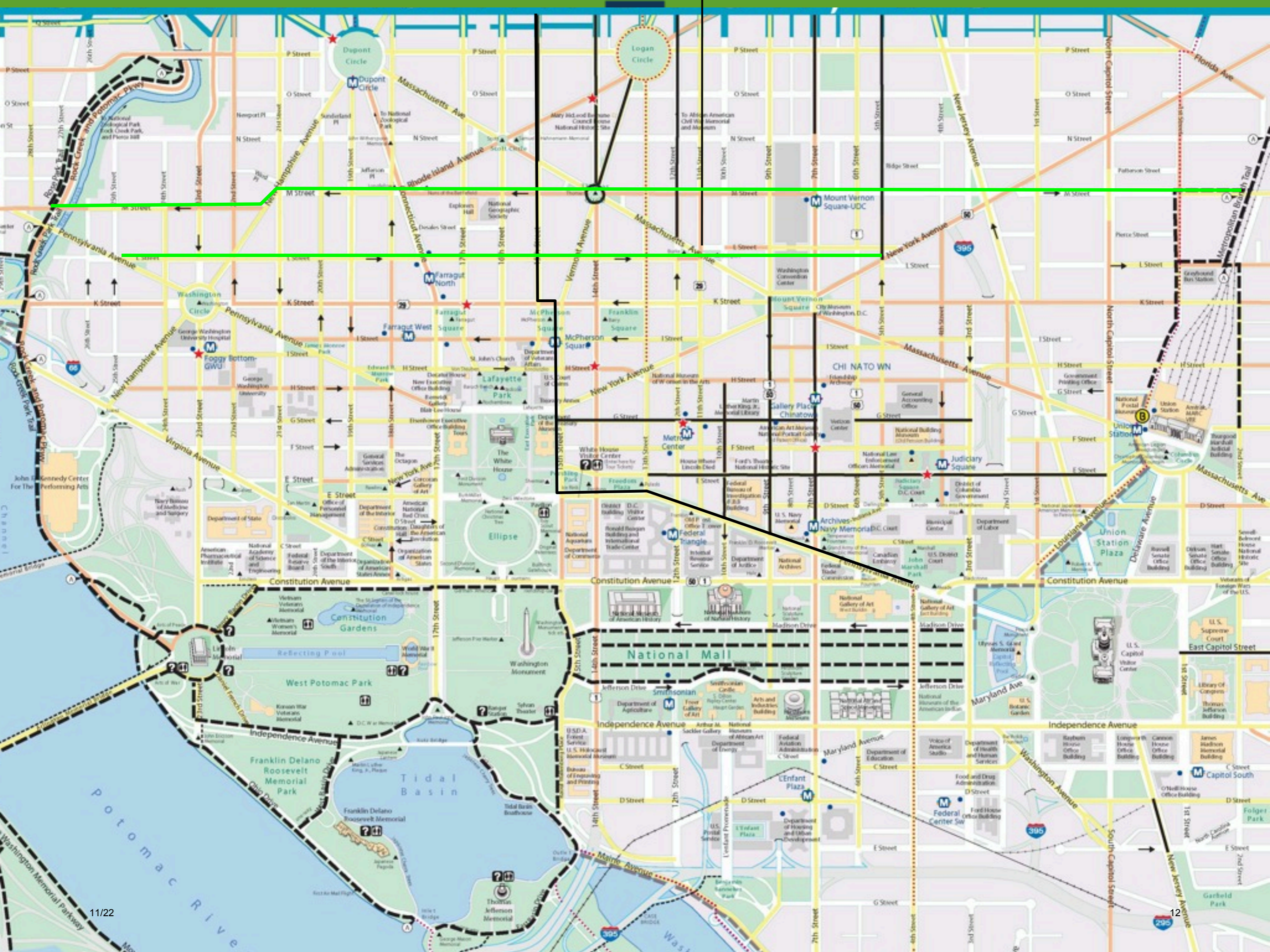
Met Branch Trail to Silver Spring

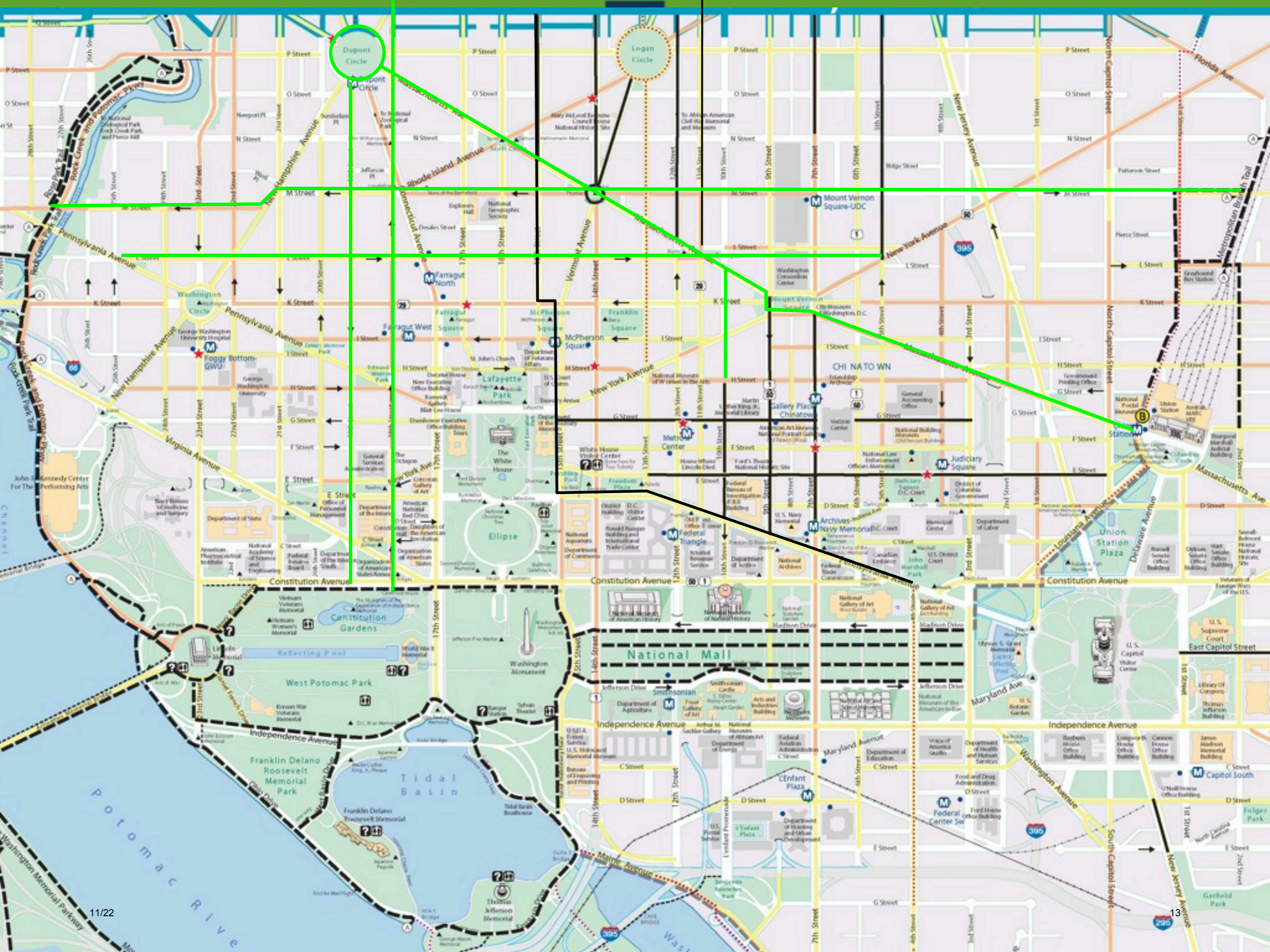
Custis Trail



Mount Vernon Trail





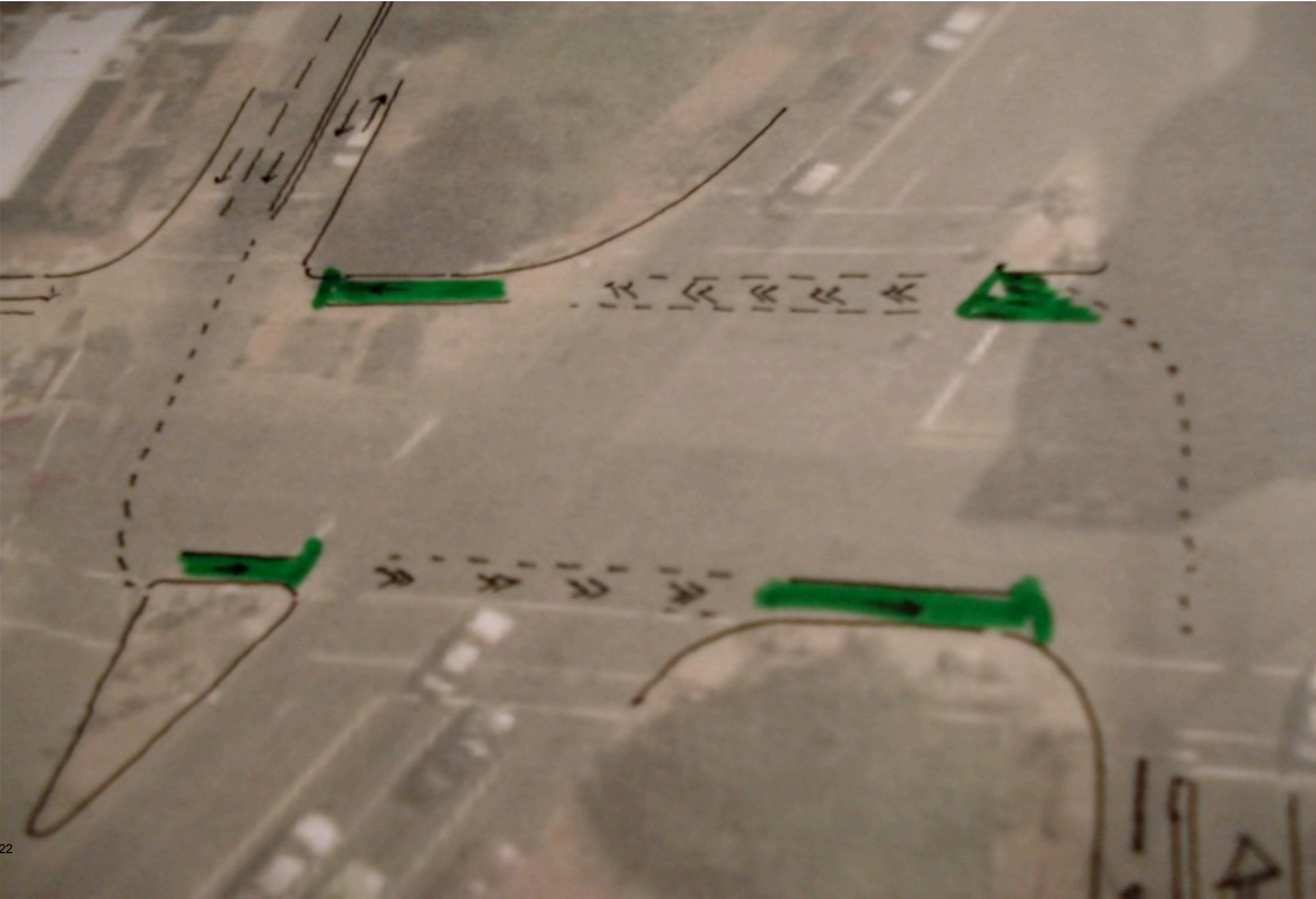


Depont Circle

Logan Circle

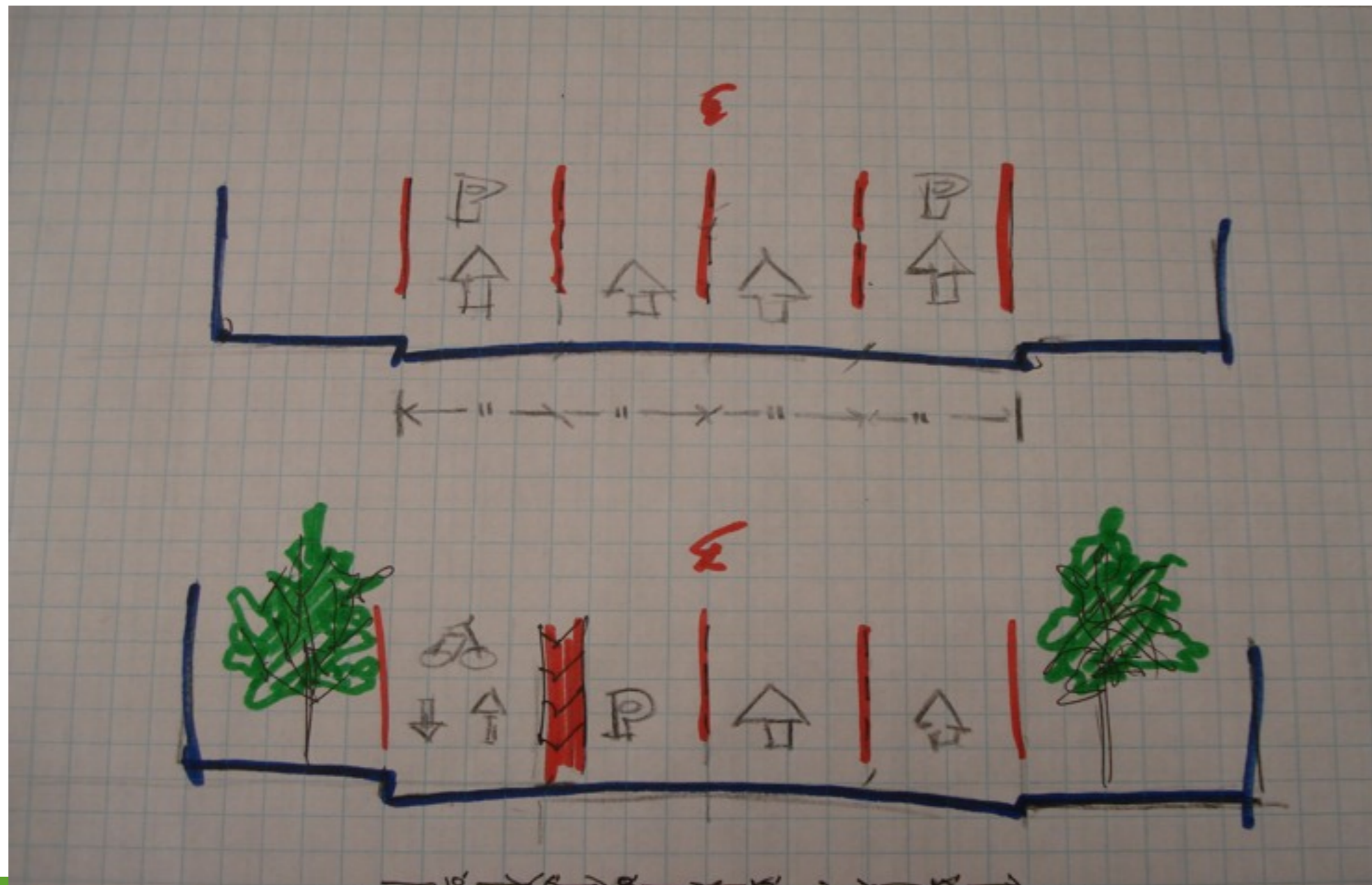


Diagonal intersection crossing (L Street @ Mass. Ave.)



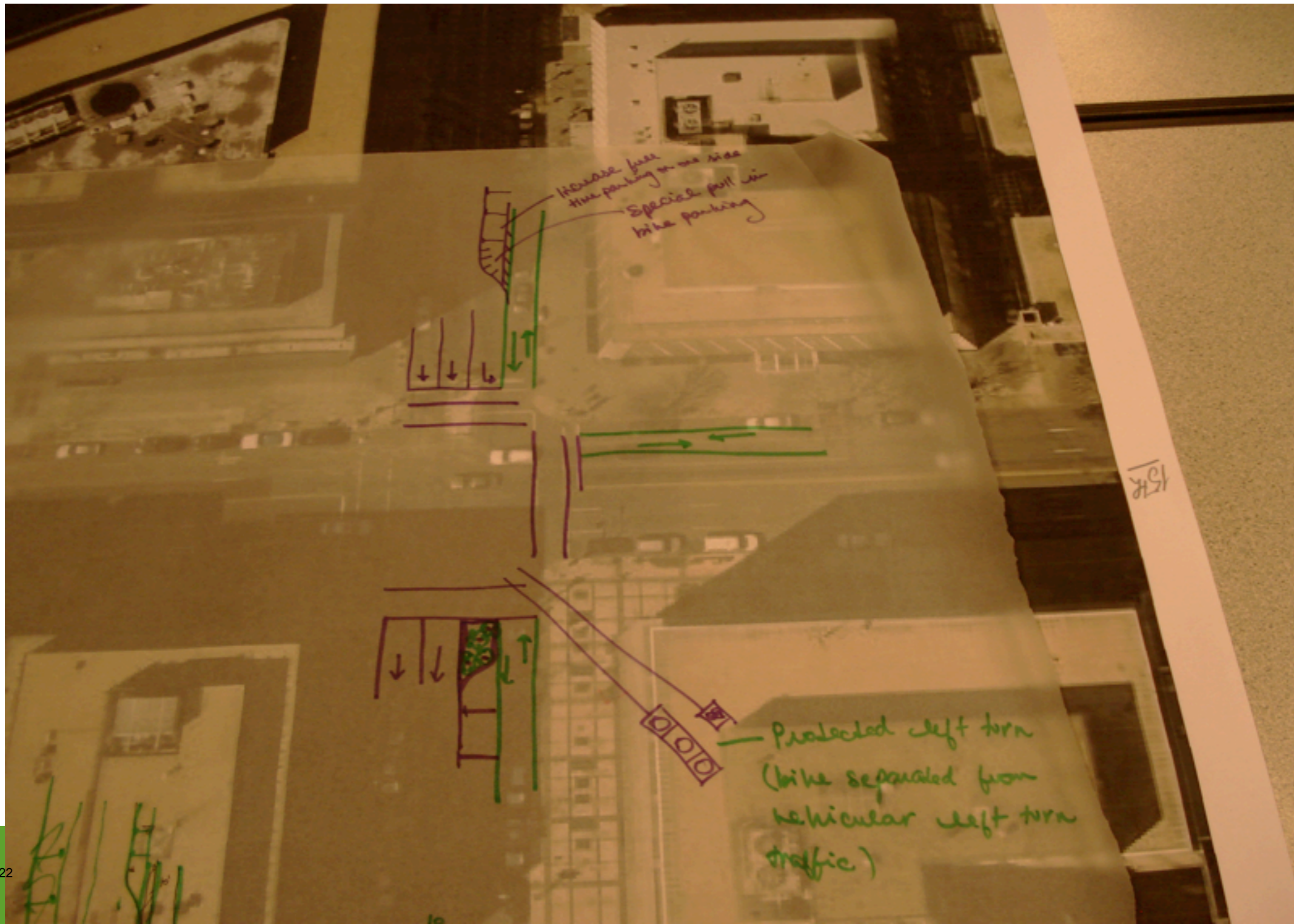


L Street cross section





L Street/15th Street – Cycle Track to Cycle Track





M Street

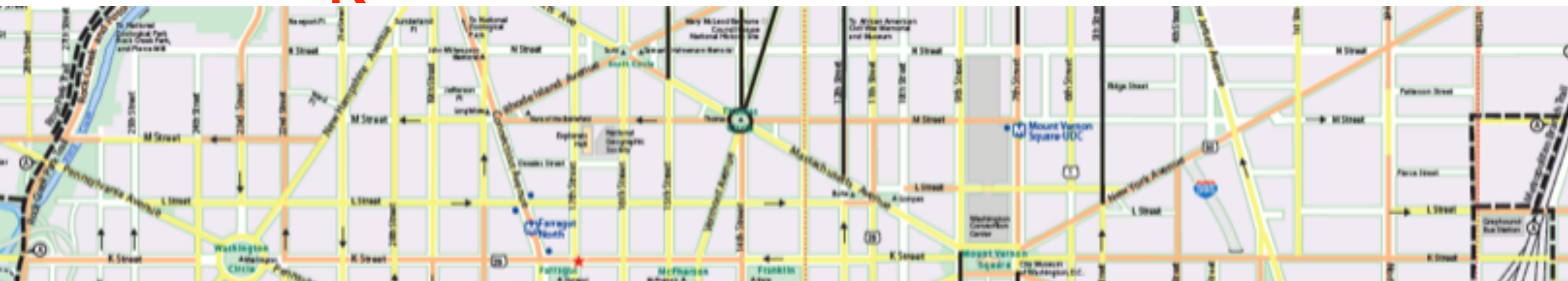




Treat M Street as sub-sections of like blocks

16-22

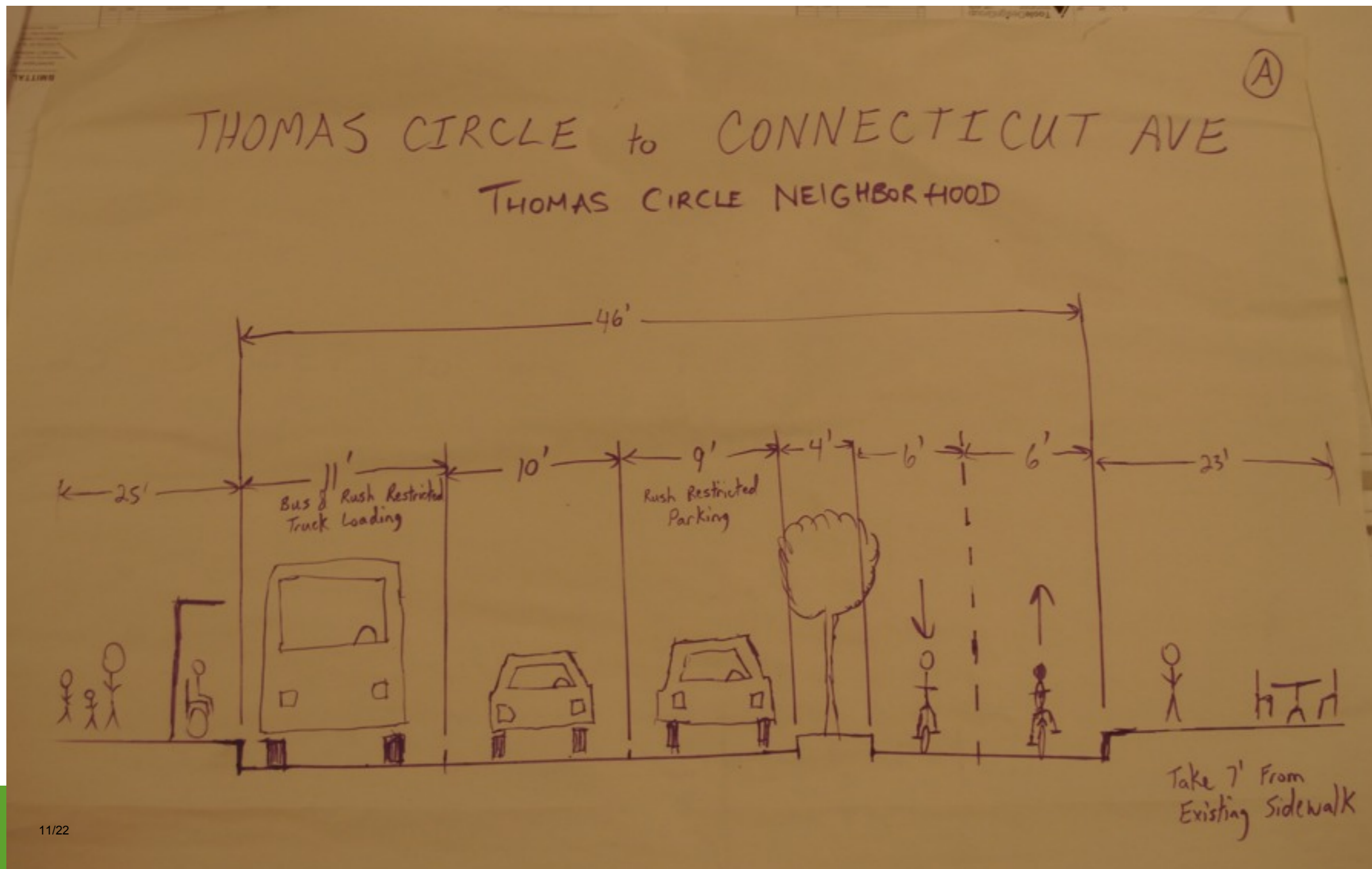
K



West End Golden Triangle
Thomas Circle

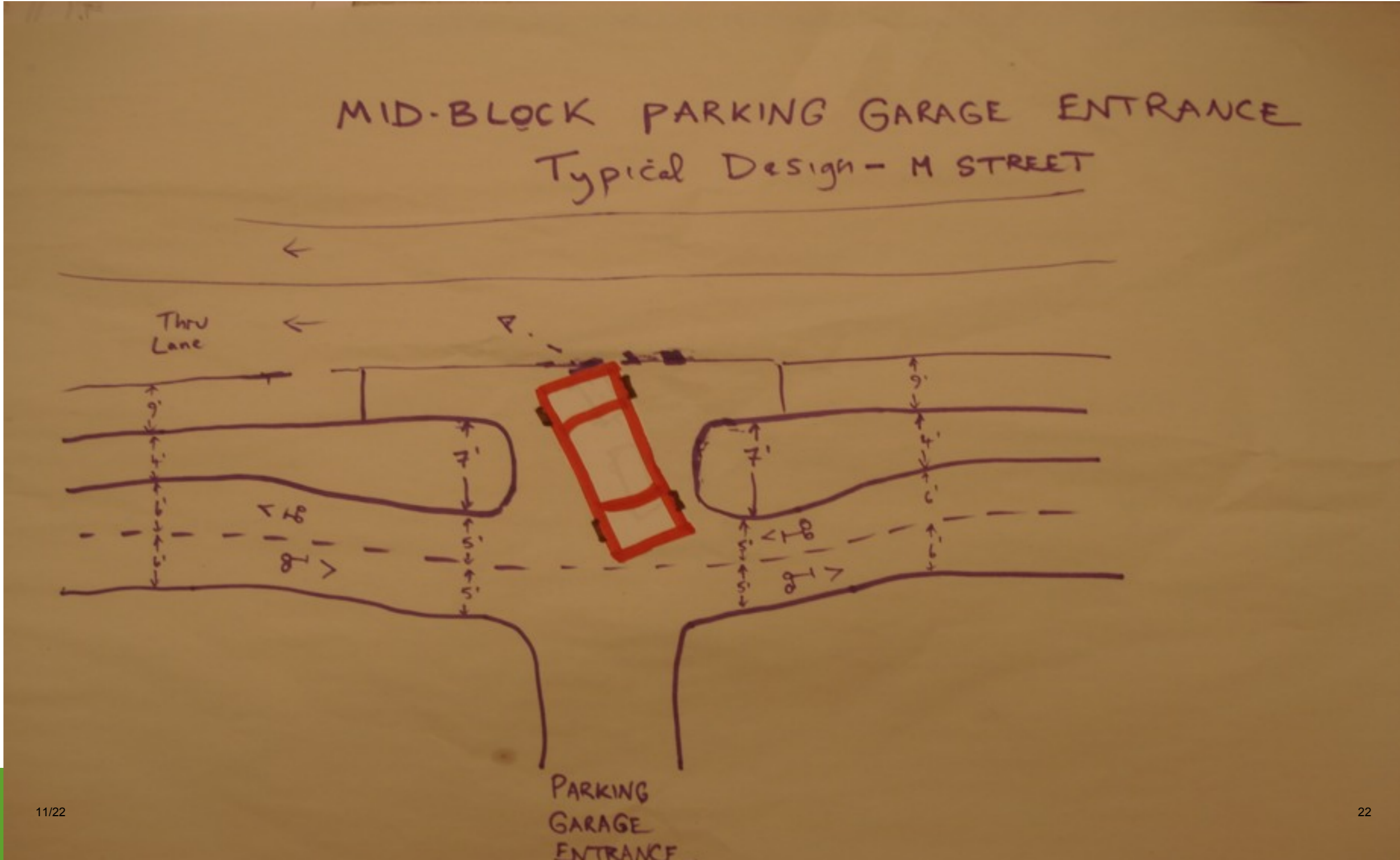


Cross section – Thomas Circle to Connecticut Ave.





Dealing with conflicts – parking garages





Issues/Areas of concern

- › Snow clearance/maintenance of way
- › Turning movements
- › Coordinating freight/delivery to manage traffic/bike conflicts
- › Accommodating parking garage entrances
- › Signalization/timing to manage oncoming traffic for contraflows



Qualities of the facility (meta-lessons)

- › Bi-directional
Dutch wisdom: “bikes flow like water”
Even in a one way facility, bicyclists will go in both directions in a separated facility
- › Located on side where outer bikes travel with direction of auto traffic
- › Be consistent with, and improve 15th St. cycletrack



Montreal cycle track with different treatment



Materials and treatments

- › Use materials and colors to differentiate bike space





Extend Dutch home zone concepts to Downtown

- Emphasize 25 mph speed limit
- Enhance public space/quality of life at key intersections with the Avenues
- Create a neighborhood zone along M Street between New Hampshire Avenue and Georgetown





Extend the amount and quality of the public space such as at M Street and Connecticut Ave.





Implementation – Begin Small

Short term

› Long term

Paint

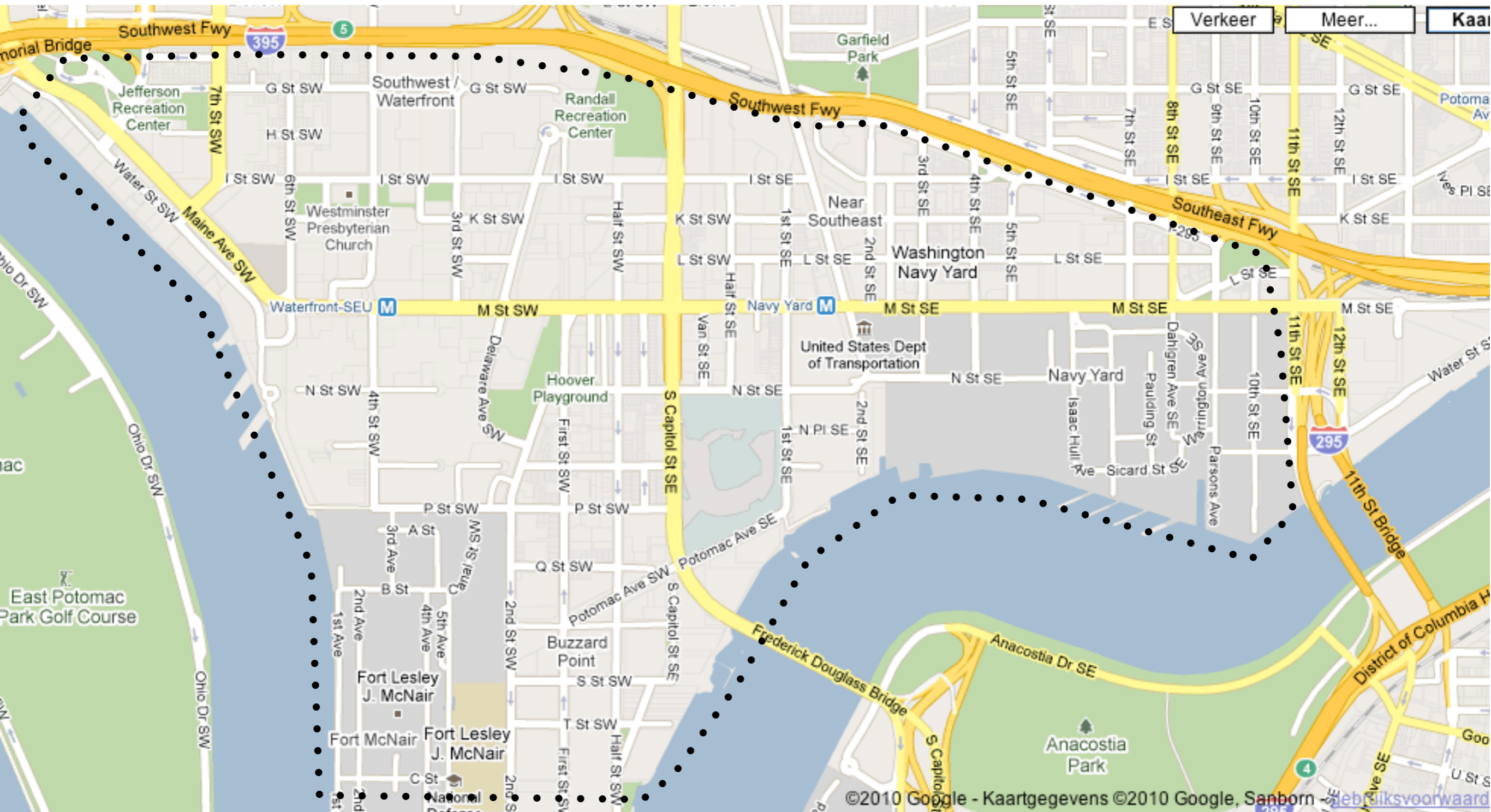
› Hard core physical treatments

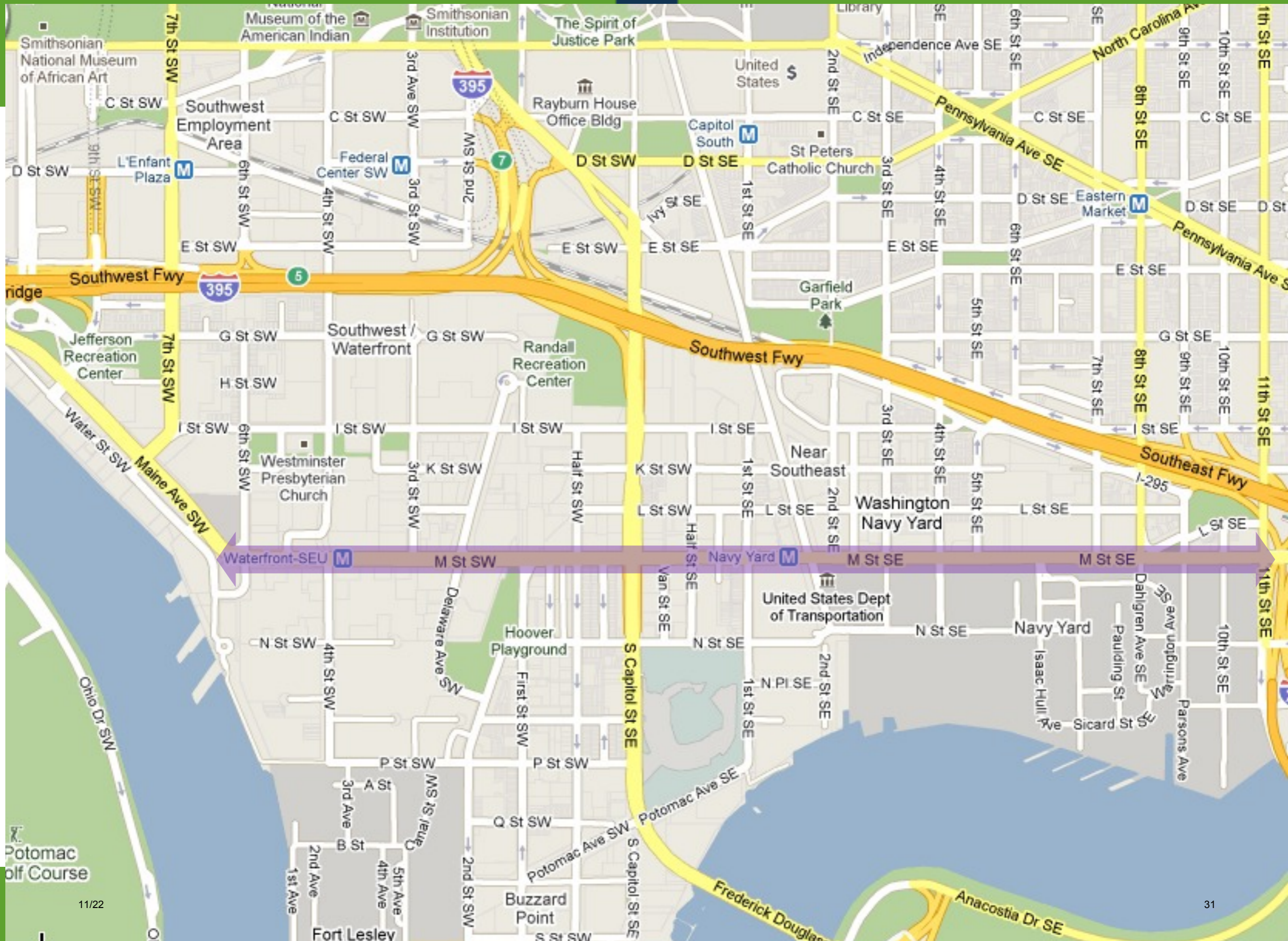
Plastic bollards

› Medians

Lane markings

› Curb and sidewalk changes







M Street Existing Conditions





M Street Recommendations

- One-way cycle track on both sides of the street
- Cycle tracks buffered by a landscape strip with street trees
- Bicycle signals at intersections with high right turn volumes
- Unique right-turn configuration
- Streetcar and stations on a median alignment



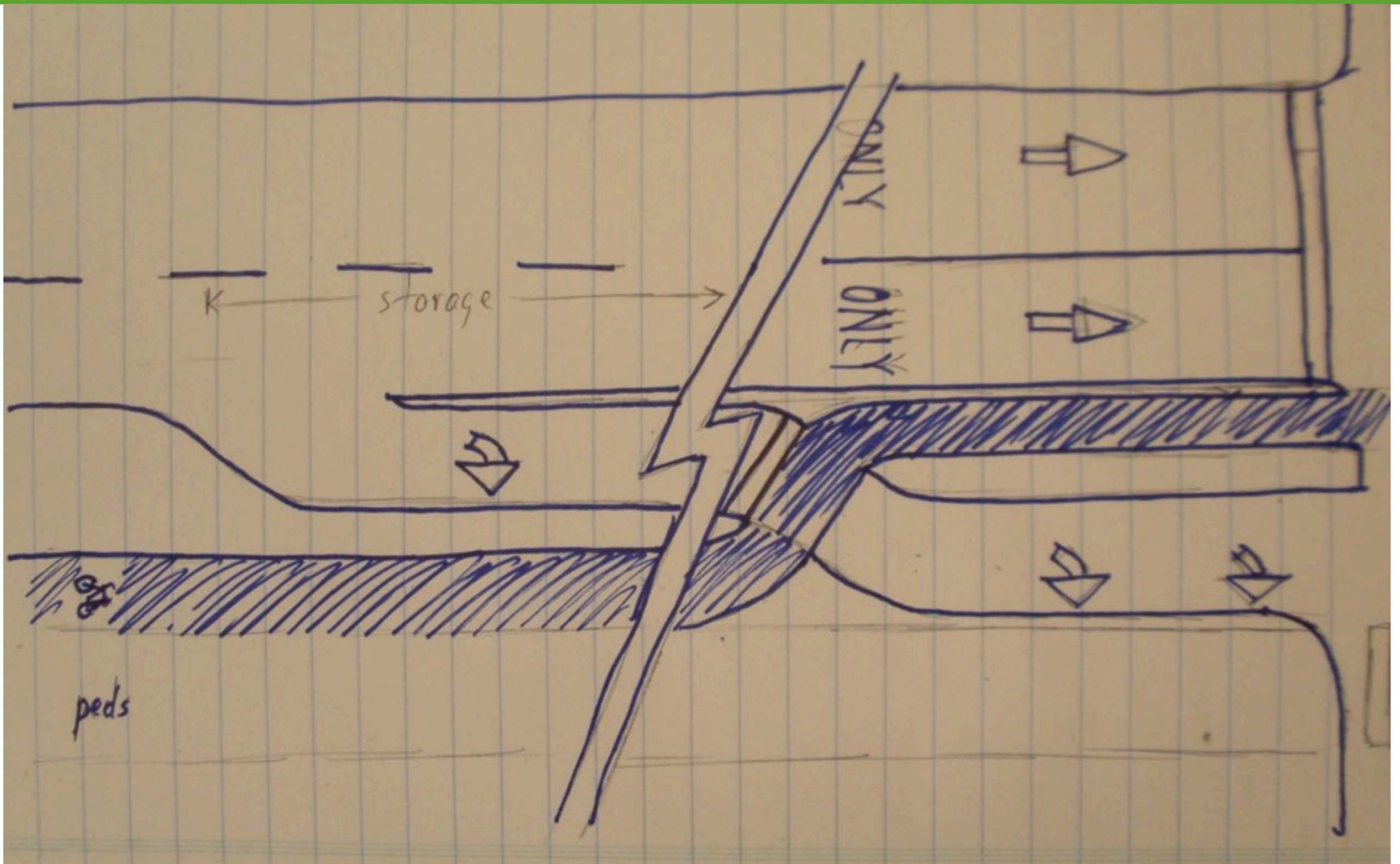
M Street Cross Section



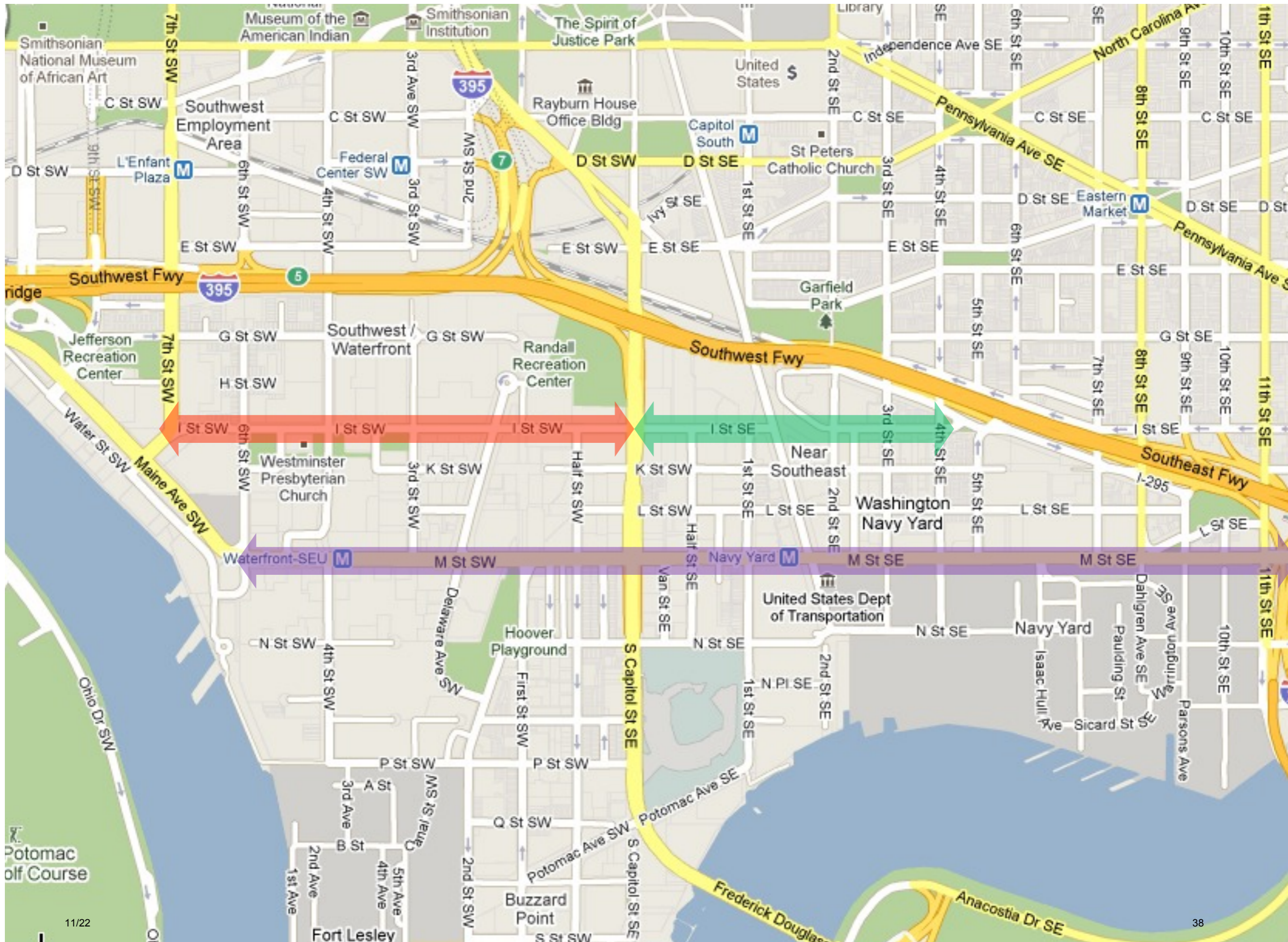


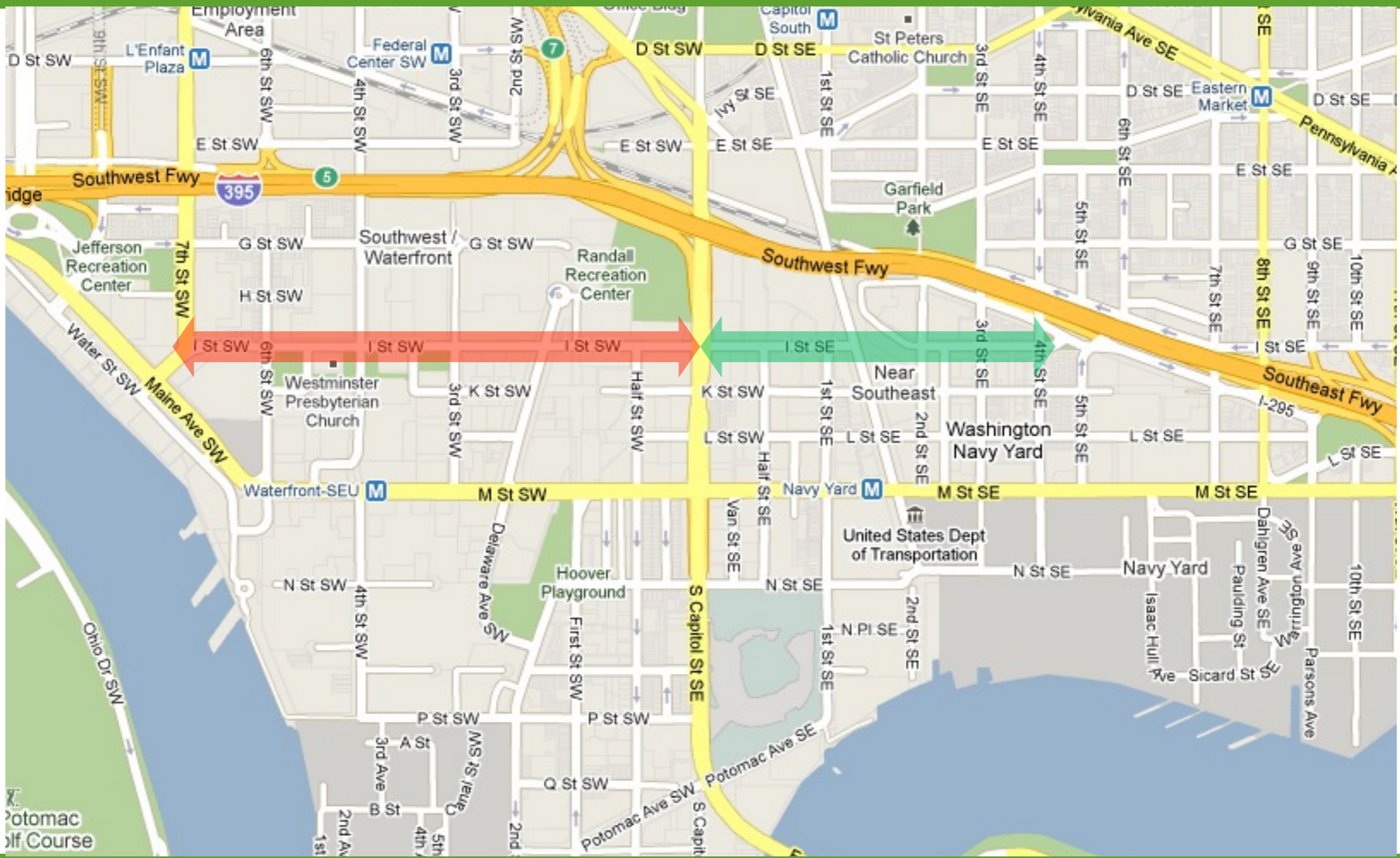
Amsterdam – Bike Lane Crossing













I Street Recommendations

- Bicycle boulevard principles
- Possibly limit through-volumes by restricting traffic every 2-3 blocks
- Traffic calming measures, for example pedestrian refuge islands
- Green wave configuration
- Option for bicycle tunnel configuration on I Street and South Capitol Street
- Colored bicycle lanes



I Street Cross Section A





I Street Existing Conditions



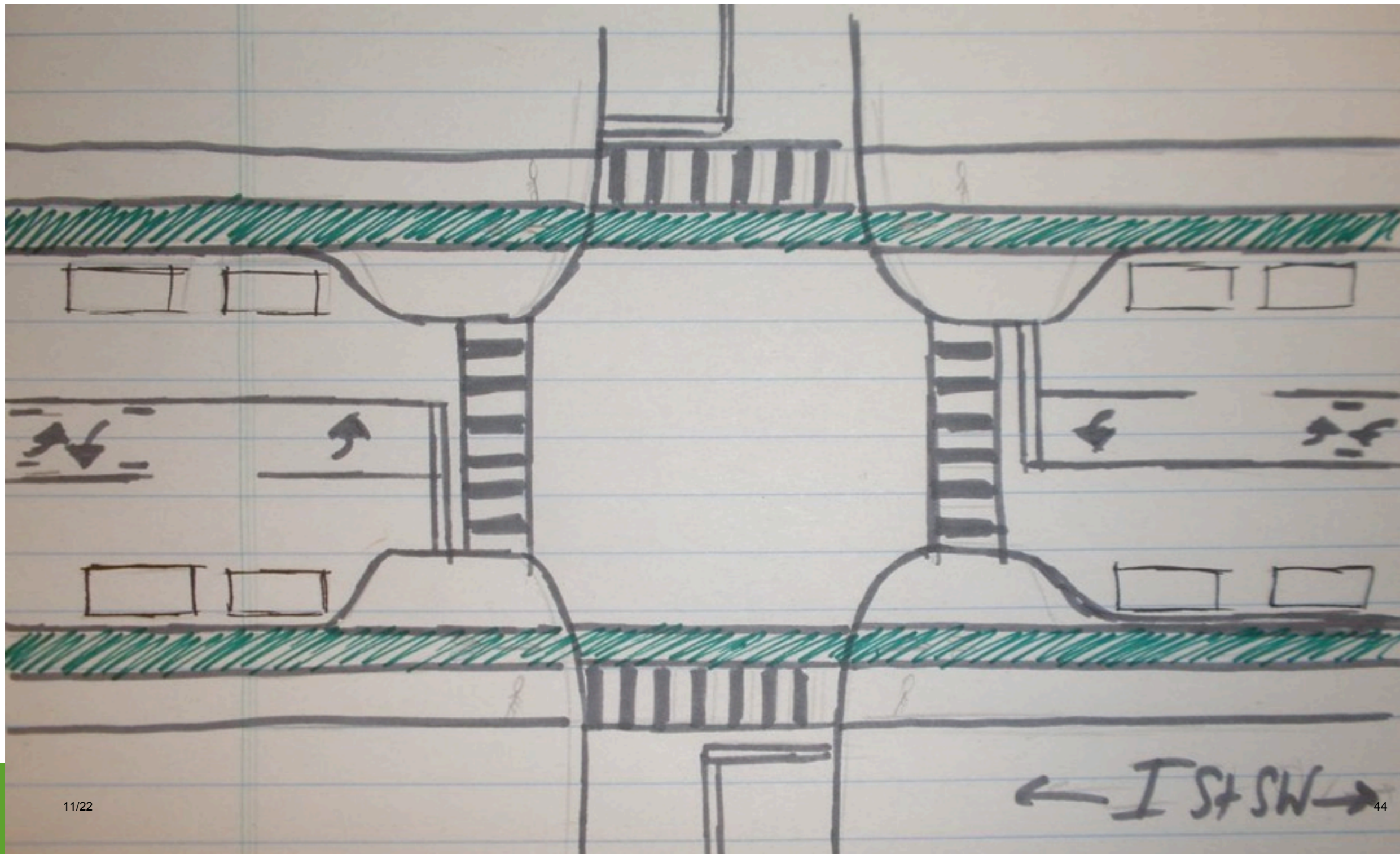


I Street SW Intersection Revision





I Street Cross Section B





I Street SE Missing Connection





I Street Connection



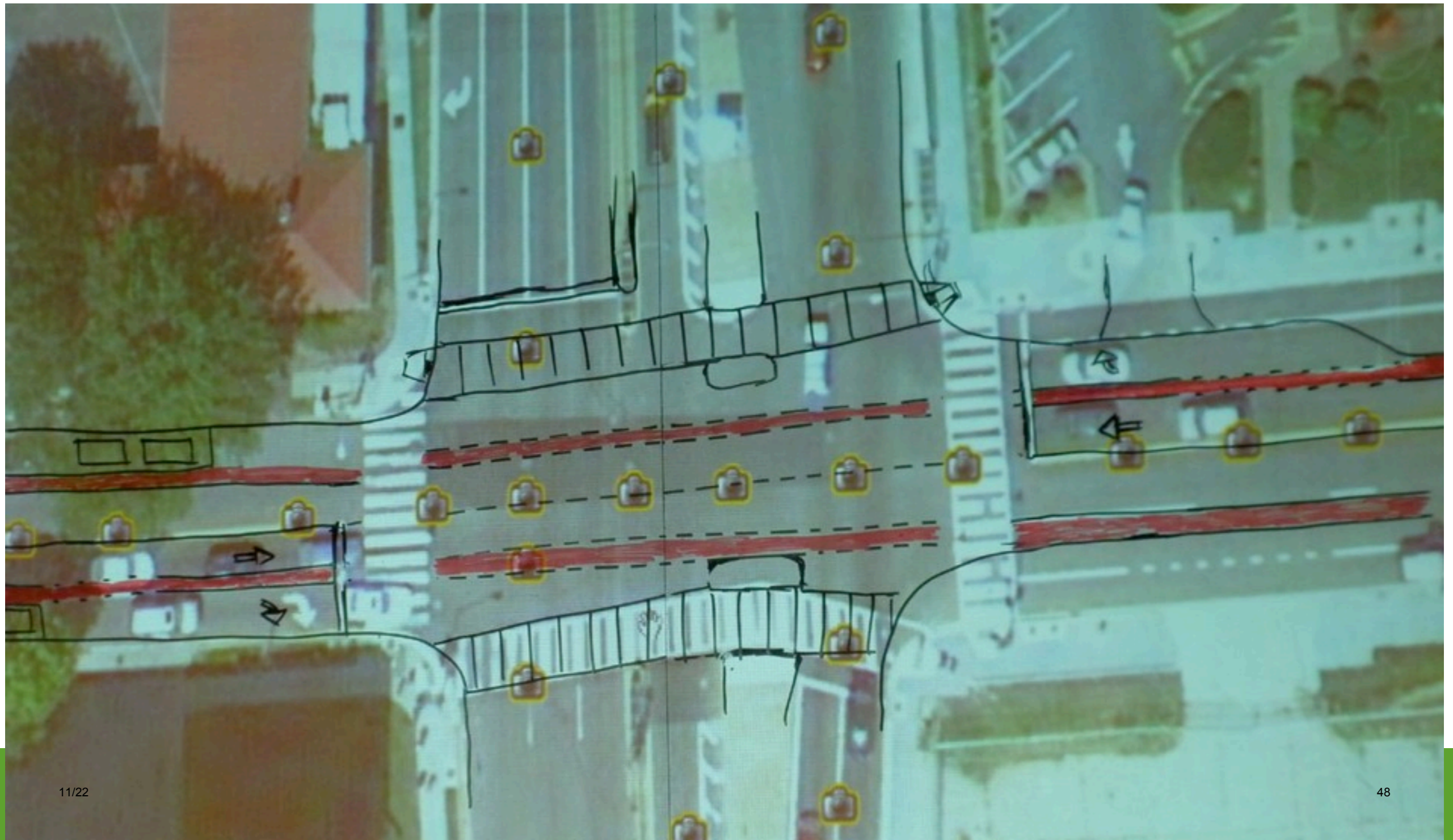


I Street & South Capitol Street





I Street & South Capitol Street

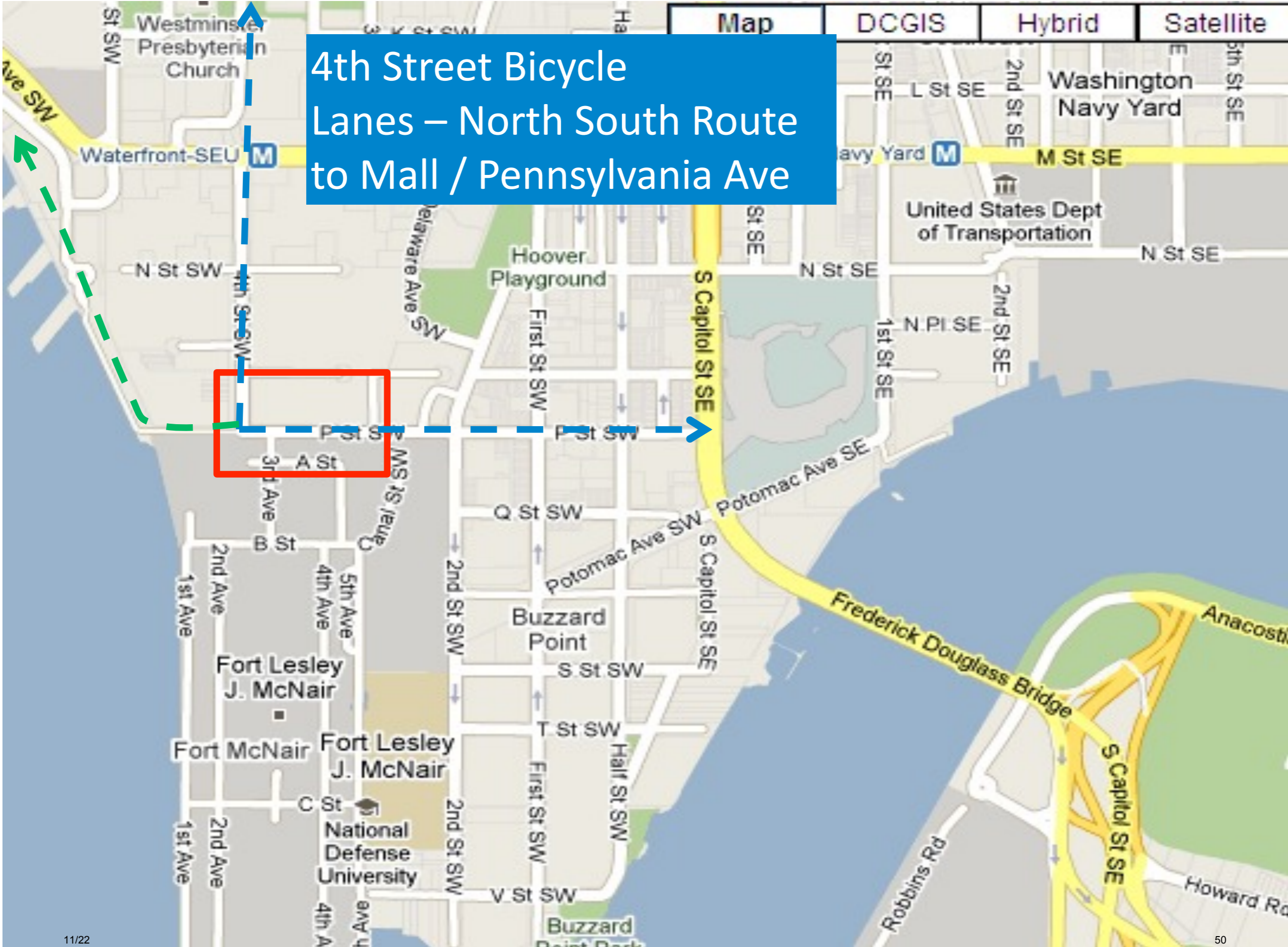


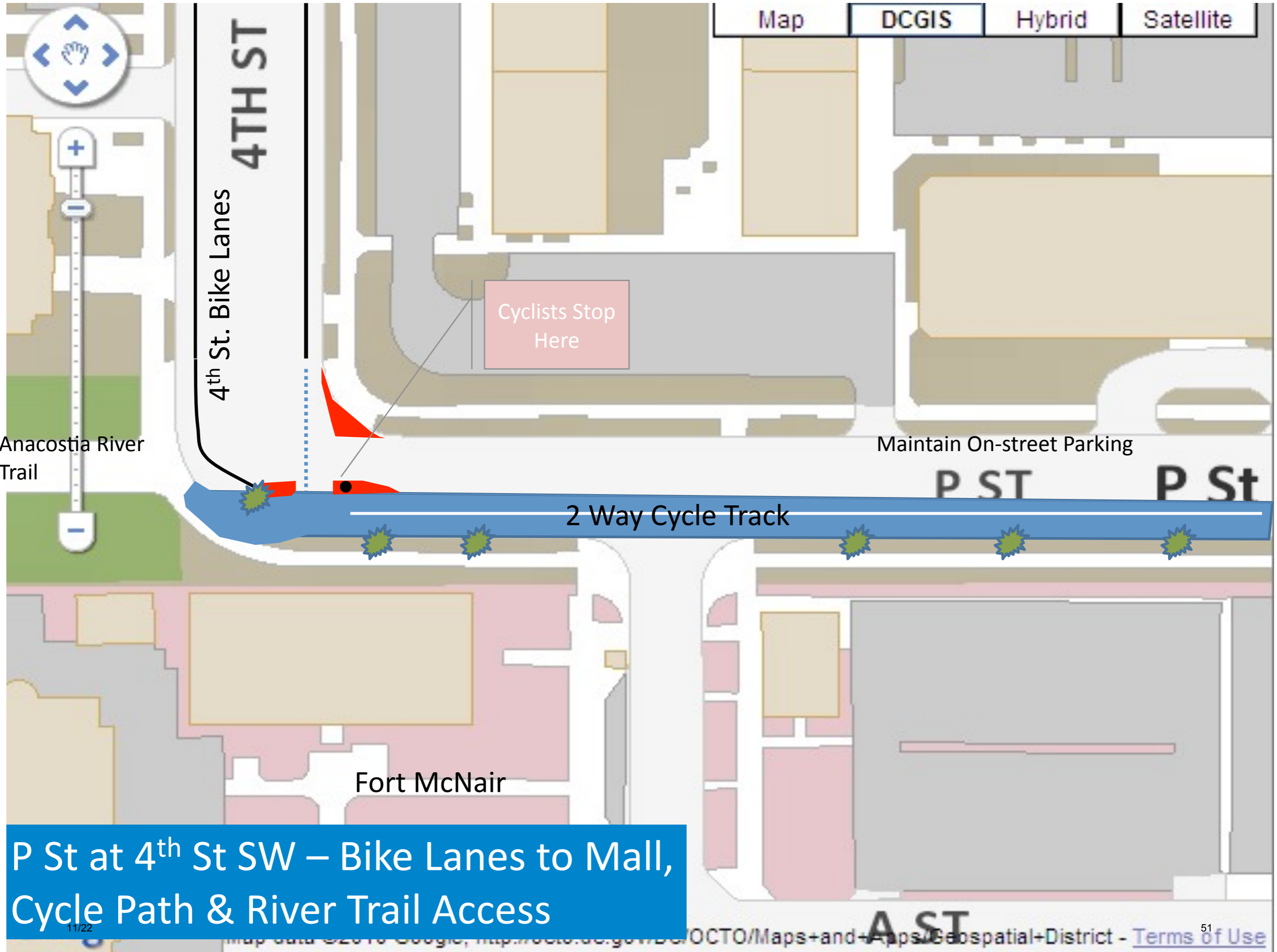


I Street & South Capitol Street



4th Street Bicycle Lanes – North South Route to Mall / Pennsylvania Ave





P St at 4th St SW – Bike Lanes to Mall, Cycle Path & River Trail Access



P^{11/22} Street SW



P Street SW – 2 Way Cycle Track with Buffer



Navy-entrances



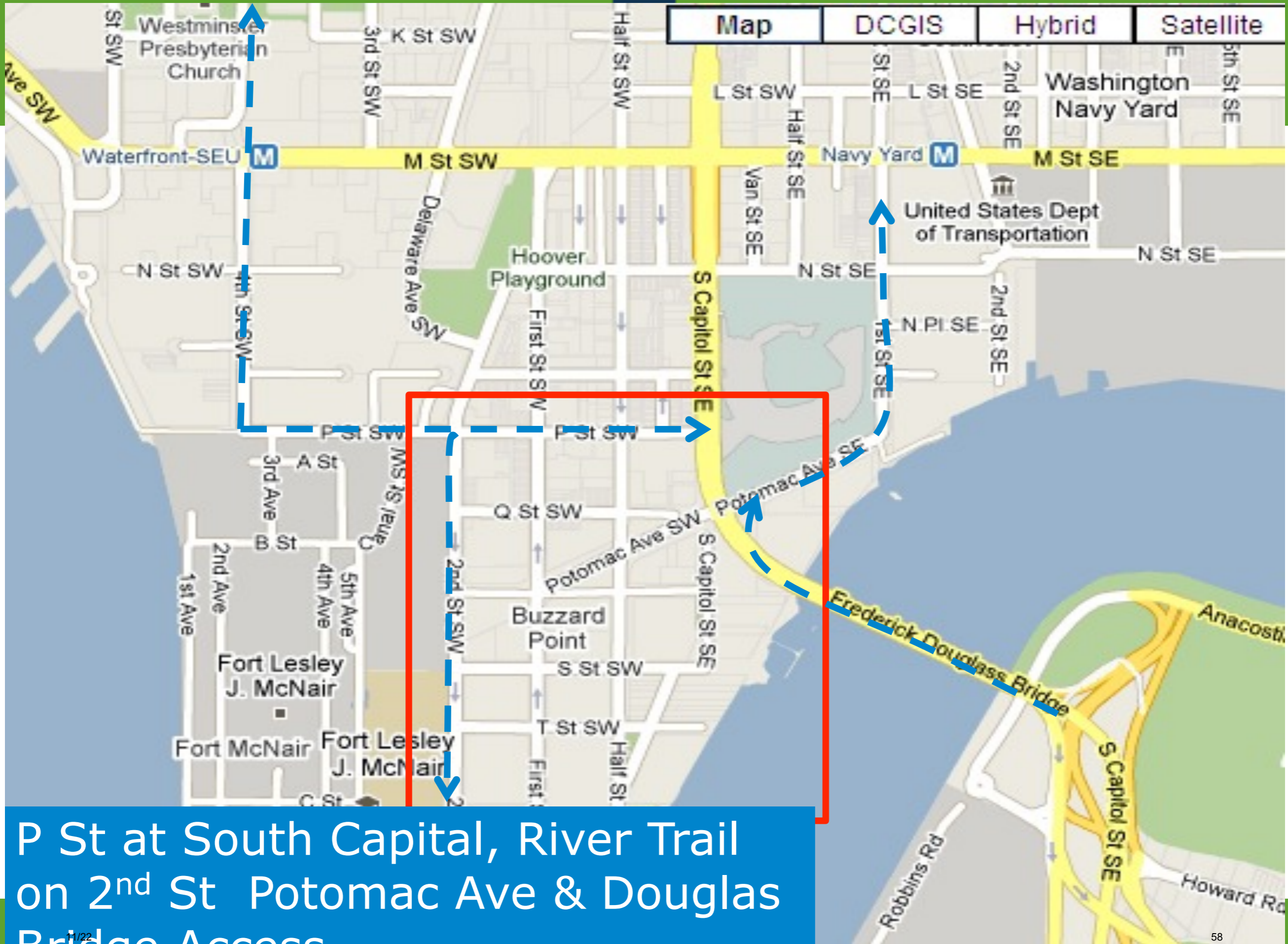




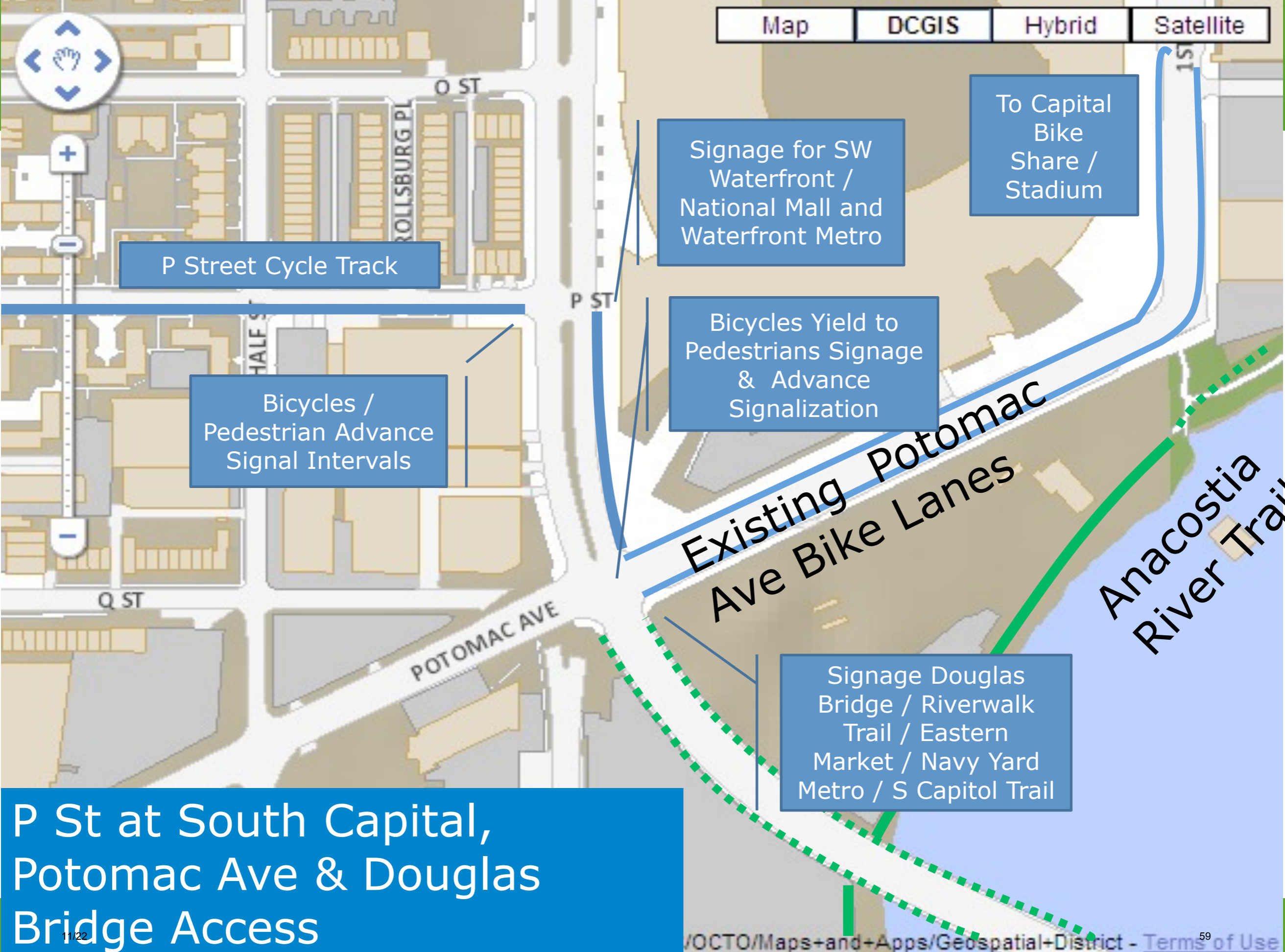
P Street SW (Bus Route west of Half St)



P Street SW - 2 Way Cycle Path with Buffer (vs lanes)



P St at South Capital, River Trail
on 2nd St Potomac Ave & Douglas
Bridge Access



P Street Cycle Track

Signage for SW Waterfront / National Mall and Waterfront Metro

To Capital Bike Share / Stadium

Bicycles / Pedestrian Advance Signal Intervals

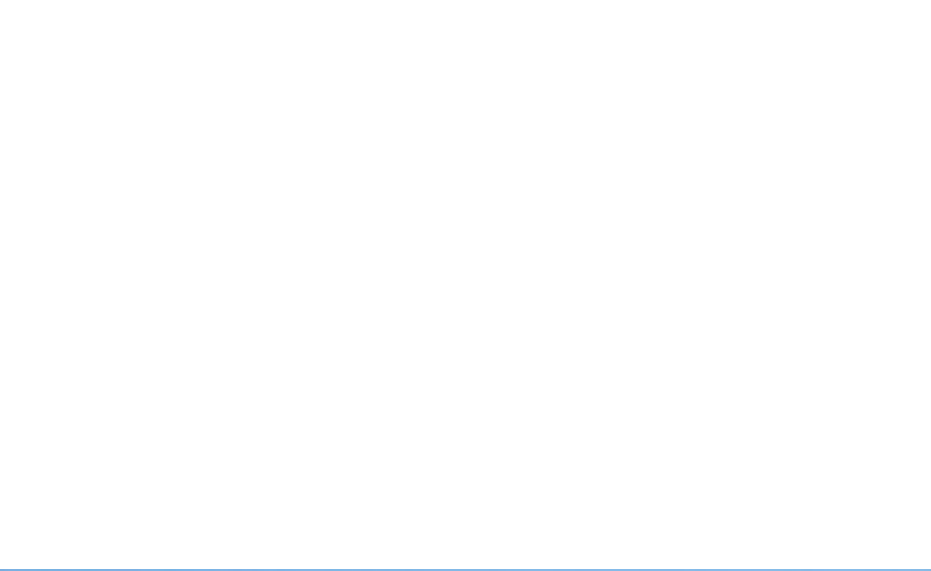
Bicycles Yield to Pedestrians Signage & Advance Signalization

Existing Potomac Ave Bike Lanes

Anacostia River Trail

Signage Douglas Bridge / Riverwalk Trail / Eastern Market / Navy Yard Metro / S Capitol Trail

P St at South Capital, Potomac Ave & Douglas Bridge Access





SW and Stadium Area Improvements

**4th Street bike lanes north of P Street to Mall/
Pennsylvania Ave.**

P Street bicycle track (River Trail to South Capital)

South Capital bicycle track (Potomac Ave to P St)

**Potomac Ave/Frederick Douglas Bridge/River Trail
access, signage connects to existing bike lanes and
new bicycle tracks**

Cycle Track as Anacostia River Trail on 2nd St

**Lead Bicycle / Pedestrian Signal Interval across
Potomac Ave & P Street at South Capitol**

Major Bicycle/Pedestrian Crossroads



ThinkBike Washington, DC/Recommendations

Kingdom of the Netherlands | Kingdom of the Netherlands | Kingdom of the Netherlands | Kingdom of the Netherlands | Kingdom of the Netherlands | Kingdom of the Netherlands | Kingdom of the Netherlands | Kingdom of the Netherlands | Kingdom of the Netherlands | Kingdom of the Netherlands





The Dutch taught us: Think BIG for Cycling

- Biking helps the environment, reduces stress, traffic, and gives you joy
- Bike infrastructure and smart street design encourages bicycling, and reduces conflict with automobiles
- Bicycle infrastructure doesn't have to be expensive – a can of paint goes a long way
- Bikes flow like water – and we should design for it
- Think about a whole bicycle network, not just two routes (M and L, NW)
- Some routes must be fabulous and have priority (green waves)
- Other routes can be bike-safe but don't require big changes
- Balancing legal liability between motorists and bicyclists encourages safety and efficiency



M and L Streets (NW) recommendations

- **M and L Streets** can become a key cross-town bike connection between existing trails and paths
- Two directions cycle path over the whole route
- In the CBD, on-street parking can be rearranged to protect cycling
- In neighborhoods, fewer lanes are needed for automobile traffic
- Right turn bike boxes at the intersections
- Innovative feature 2-stage left turn box
- Colored pavement for cycle path at the intersections
- Special signal phasing for cyclist



I Street recommendations

1. Bicycle boulevard principles
2. Possibly limit through-volumes by restricting traffic every 2-3 blocks
3. Traffic calming measures, for example pedestrian refuge islands
4. Green wave configuration
5. Option for bicycle tunnel configuration on I Street and South Capitol Street
6. Colored bicycle lanes



M Street recommendations

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Southwest and Stadium Area Improvements

- 4th Street bike lanes north of P Street to Mall/Pennsylvania Ave.
- P Street bicycle track (River Trail to South Capital)
- South Capital bicycle track (Potomac Ave to P St)
- Potomac Ave/Frederick Douglas Bridge/River Trail access, signage connects to existing bike lanes and new bicycle tracks
- Cycle Track as Anacostia River Trail on 2nd St
- Lead Bicycle / Pedestrian Signal Interval across Potomac Ave & P Street at South Capitol
- Major Bicycle/Pedestrian Crossroads



Outreach Ideas

1. Target people who do not bike yet
2. “Try a bike” class (WABA, bike shops, DDOT)
3. Work with churches, health centers, youth centers, doctors, nurses

MONTGOMERY COLLEGE
Workforce Development & Continuing Education
TRANSPORTATION SAFETY INSTITUTE

**Cycling Commuter
Safety and Traffic Skills**

Course is comprised of League of American Bicyclist safety practices and supports the Bicycle Commuter Act.

BIC001: Cycling Commuter Safety and Traffic Skills

Learn smart cycling skills to enrich your ride. This NEW cycling course offers techniques for commuting by bicycle. Build greater confidence in maneuvering safely and legally in traffic, on trails, roads, paths and in groups. This course will cover instruction on bicycle selection and fit, bike handling, efficient pedaling, breakdown repairs, and equipment use and care. This course is comprised of both classroom time and on-road practice time with a supervised group ride through local neighborhoods. Participants must bring their own bike, helmet, and water. Participants will be required to sign liability waivers.

\$69.00 + \$30.00 Fee = \$99.00;
Non-MD Residents: Add \$50.00

Rockville Campus	Room: 2165D
CRN#: 41108	2 sessions
5/14-5/15	F, S
6:30 p.m.–8:30 p.m. (F)	
9:00 a.m.–3:30 p.m. (S)	

For more information: call 240-567-5188
or visit us online at
www.montgomerycollege.edu/wdca/safety.html

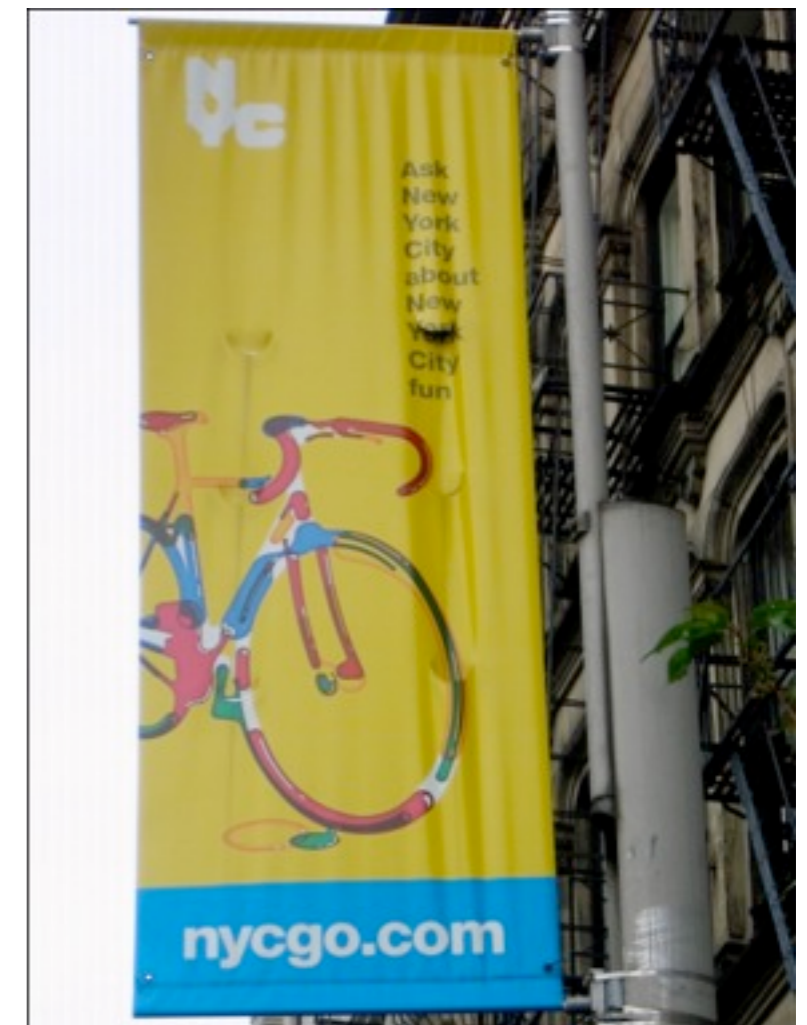
Registration Form on Back of Flyer

4/29/13



Outreach Ideas

1. Provide free trial on Capital Bikeshare or discount annual membership
2. Hold classes and do outreach at Universities, community colleges
3. Target areas around new bikeways
4. Bike education at schools
5. Banners





Education Ideas

More/Better Driver training

(In the Netherlands, 18 years old, 40 classes, 5 questions on bicycling on exam)

Colored bike lanes

Radio ads, internet videos

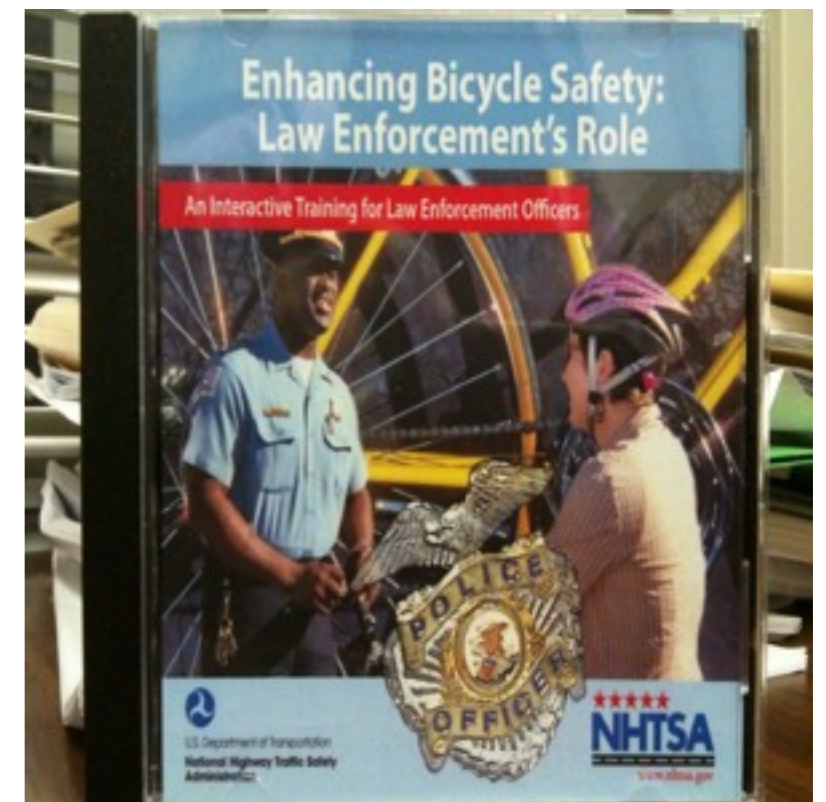
Reduce speeding (enforcement, traffic calming)





Enforcement

1. Lights (for bicyclists)
2. Citations to cars (speeding)
3. More police on bikes
4. Waive ticket fee for bicyclists who if they agree to take a class
5. Points on your drivers license for bicycle violations





Other Policies

1. More/better wayfinding signs
2. Make driving/parking more expensive
 - In NL, gas is \$9/gallon
 - In NL, parking is \$20/day
3. More surveys/counting of bicyclists

